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Anthony Lee Washington III

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HDR ENGINEERING, INC.  
120 BRENTWOOD COMMONS WAY  
SUITE 525  
BRENTWOOD, TN 37027  
ANTHONY L. WASHINGTON III P.E. 119749

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

| SHEET NAME  | SHEET NO.     |
|---|---------------|
| SIGNATURE SHEET .....                                 | ROADWAY-SIGN1 |
| TITLE SHEET .....                                     | 1             |
| ROADWAY INDEX, STANDARD ROADWAY DRAWINGS AND STANDARD |               |
| TRAFFIC DESIGN DRAWINGS .....                         | 1A            |
| PROJECT COMMITMENTS .....                             | 1B            |
| ESTIMATED ROADWAY QUANTITIES .....                    | 2             |
| TYPICAL SECTIONS AND PAVEMENT SCHEDULE .....          | 2B            |
| GENERAL NOTES.....                                    | 2C            |
| SPECIAL NOTES.....                                    | 2D            |
| ENVIRONMENTAL NOTES.....                              | 2E, 2E1       |
| TABULATED QUANTITIES .....                            | 2F            |
| UTILITY NOTES AND UTILITY OWNERS.....                 | 3             |
| PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL..... | 4             |

# SIGNATURE SHEET



Index Of Sheets  
SEE SHEET NO. 1A

PROJECT TO BE LET WITH:  
DECATUR CO. SR-201  
PIN 133153.00

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
BUREAU OF ENGINEERING

HENDERSON COUNTY

SR-201  
FROM: L.M. 4.60 (SR-104)  
TO: L.M. 14.38 (DECATUR COUNTY LINE)

RESURFACE & SAFETY  
SCRUB SEAL, 307 CW, GUARDRAIL, & PAVEMENT MARKINGS

STATE HIGHWAY NO. 201 F.A.H.S. NO. N/A

END PROJECT NO. 39S201-S8-004 RESURFACE & SAFETY  
L.M. 14.38 (DECATUR COUNTY LINE)

LIMIT OF RESURFACING  
SR-201 (L.M. 10.97) =  
SR-114 (L.M. 6.52)

SR-201 RUNS ALONG SR-114 FOR 6.40 MILES  
(NO RESURFACING WITHIN THIS  
SECTION UNDER THIS PROJECT)

LIMIT OF RESURFACING  
SR-201 (L.M. 10.97) =  
SR-114 (L.M. 0.12)

BRIDGE-DECK-REPAIR PROJECT NO. 39S201-M3-003  
SR-201 L.M. 6.97 (39S81350007)

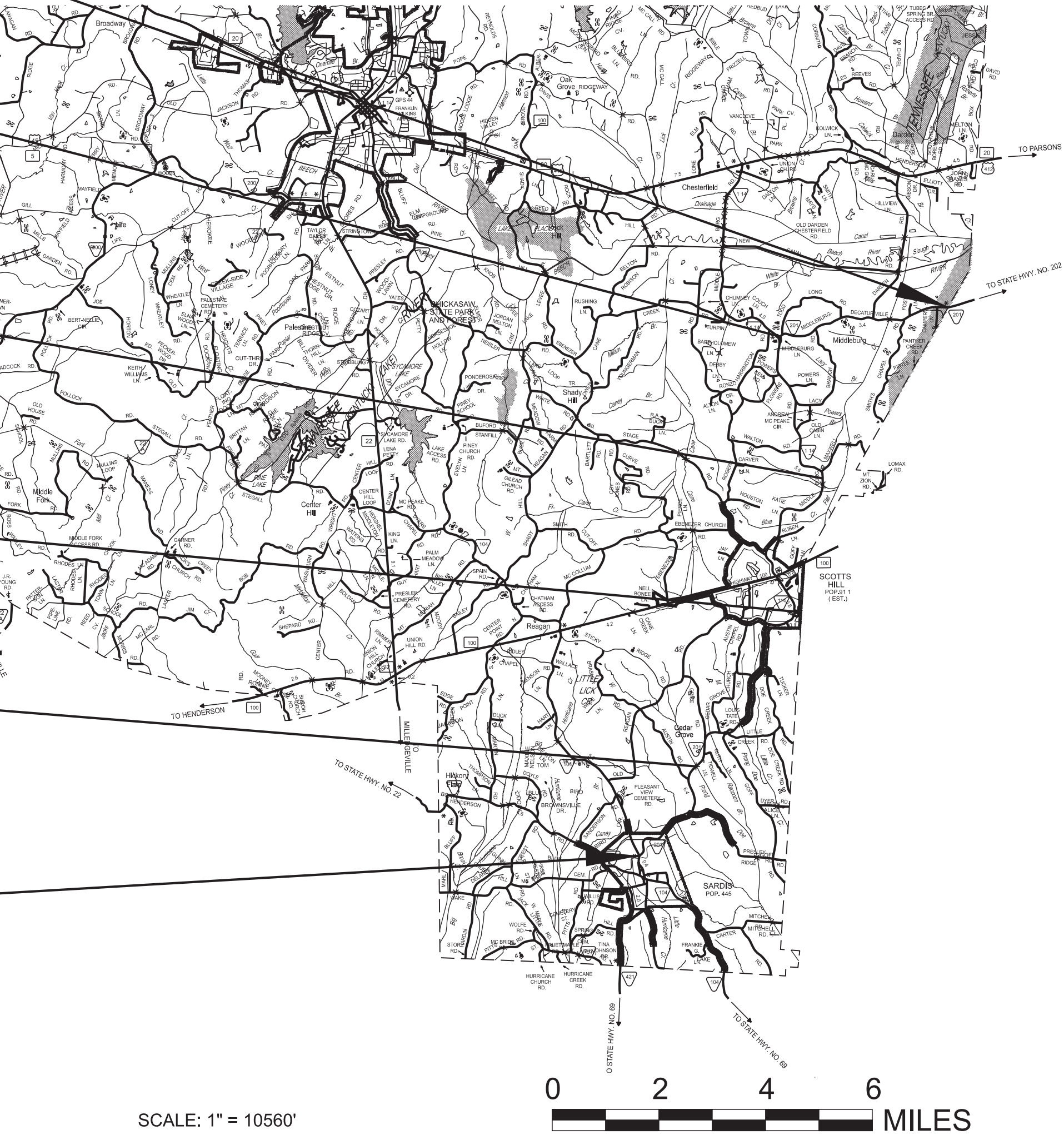
BEGIN PROJECT NO. 39S201-S8-004 RESURFACE & SAFETY  
L.M. 4.60 (SR-104)

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES  
CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW  
THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF  
THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND  
ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS  
AND IN THE PROPOSAL CONTRACT.

TDOT PROJECT MANAGER : LYNN EVANS, P.E., REG. 4  
DESIGNED BY : HDR ENGINEERING, INC.  
DESIGNER : ANTHONY L. WASHINGTON, III, P.E. CHECKED BY DAVID HORNE, P.E.  
P.E. NO. 98043-4283-04  
PIN NO. 133165.00

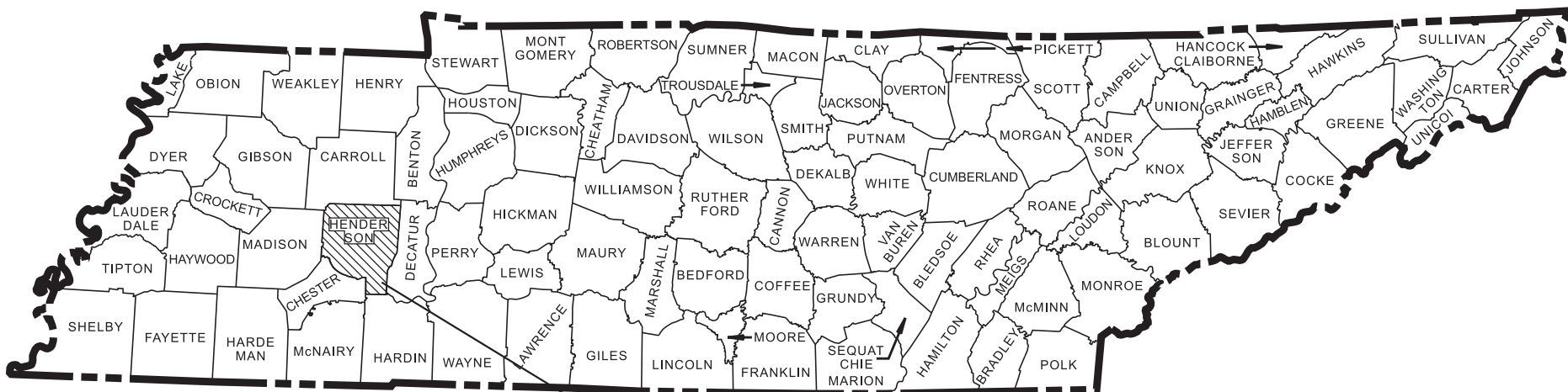


SCALE: 1" = 10560'

PROJECT LENGTH 9.78 MILES  
TOTAL LANE MILES RESURFACED 19.56 MILES

|   |     |      |
|---|-----|------|
| DOES THIS PROJECT QUALIFY<br>FOR UTILITY CHAPTER 86 | YES | NO X |
| WORK ZONE SIGNIFICANCE DETERMINATION                |     |      |
| SIGNIFICANT   | YES | NO X |

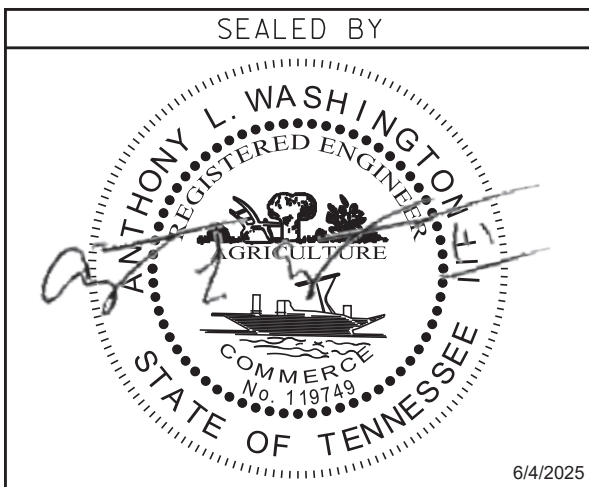
|                 |               |           |
|-----------------|---------------|-----------|
| TENN.           | YEAR          | SHEET NO. |
|                 | 2025          | 1         |
| STATE PROJ. NO. | 39S201-S8-004 |           |
| STATE PROJ. NO. | 39S201-M3-003 |           |



PROJECT LOCATION

BRIDGE ID. # 39S81350005 39S81350007 39S81350009  
39016980001

NO EXCLUSIONS



APPROVED: WILL REID, DEPUTY COMMISSIONER / CHIEF ENGINEER

DATE:

APPROVED: HOWARD H. ELEY, DEPUTY GOVERNOR & COMMISSIONER

| TRAFFIC COUNTER & WEATHER STATIONS |          |
|------------------------------------|----------|
| STATION LOCATION                   | LOG MILE |
| TC STATION 59                      | 5.517    |
| TC STATION 47                      | 11.504   |

| TRAFFIC DATA             |        |
|--------------------------|--------|
| ADT (2025)               | 564    |
| POSTED SPEED LIMITS      |        |
| L.M. 4.60 TO L.M. 8.35   | 45 MPH |
| L.M. 8.35 TO L.M. 10.45  | 40 MPH |
| L.M. 10.45 TO L.M. 10.97 | 30 MPH |
| L.M. 10.97 TO L.M. 14.38 | 45 MPH |

U.S. DEPARTMENT OF TRANSPORTATION  
FEDERAL HIGHWAY ADMINISTRATION

APPROVED: DIVISION ADMINISTRATOR DATE



ROADWAY INDEX

| SHEET NAME  | SHEET NO.     |
|---|---------------|
| SIGNATURE SHEETS.....   | ROADWAY-SIGN1 |
| TITLE SHEET .....   | 1             |
| ROADWAY INDEX, STANDARD ROADWAY DRAWINGS AND STANDARD TRAFFIC DESIGN DRAWINGS ..... | 1A            |
| PROJECT COMMITMENTS .....   | 1B            |
| ESTIMATED ROADWAY QUANTITIES .....  | 2             |
| TYPICAL SECTIONS AND PAVEMENT SCHEDULE .....  | 2B            |
| GENERAL NOTES.....  | 2C            |
| SPECIAL NOTES.....  | 2D            |
| ENVIRONMENTAL NOTES.....  | 2E, 2E1       |
| TABULATED QUANTITIES .....  | 2F            |
| UTILITY NOTES, AND UTILITY OWNERS.....  | 3             |
| PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL.....                               | 4             |
| BRIDGE SHEETS.....  | B-1           |
| NO UTILITY SHEETS INCLUDED IN THIS SET OF PLANS.                                    |               |

STANDARD ROADWAY DRAWINGS


| DWG.   | REV.     | DESCRIPTION   |
|--|----------|---|
| STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS |          |   |
| RD-A-1   | 02-20-20 | STANDARD ABBREVIATIONS A THROUGH L                    |
| RD-A-2   |          | STANDARD ABBREVIATIONS M THROUGH Z                    |
| RD-L-1   | 02-20-20 | STANDARD LEGEND                                       |
| RD-L-1A  |          | STANDARD LEGEND                                       |
| SAFETY DESIGN AND GUARDRAILS                             |          |   |
| S-GRS-4  | 05-04-22 | SPECIAL CASE GUARDRAIL HEIGHT TRANSITION DETAIL       |
| S-GRT-2  | 06-28-19 | TYPE 38 GUARDRAIL END TERMINAL                        |
| S-GRT-2R   | 06-28-19 | EARTH PAD FOR TYPE 38 AND TYPE 21 TERMINAL (RETROFIT) |
| S-GRT-3  | 06-28-19 | TYPE 21 GUARDRAIL END TERMINAL                        |
| S-GRA-4  | 03-01-23 | IN-LINE GUARDRAIL ANCHOR TO PRIVATE DRIVE             |

STANDARD TRAFFIC DESIGN DRAWINGS

| DWG.                     | REV.     | DESCRIPTION   |
|--------------------------|----------|---|
| DESIGN - TRAFFIC CONTROL |          |   |
| T-M-1                    | 01-24-25 | DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS |
| T-M-4                    | 01-24-25 | STANDARD INTERSECTION PAVEMENT MARKINGS                                       |
| T-WZ-10                  | 03-26-25 | ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS                            |

| TYPE   | YEAR | PROJECT NO.   | SHEET NO. |
|--------|------|---------------|-----------|
| RESURF | 2025 | 39S201-S8-004 | 1A        |
|        |      |               |           |
|        |      |               |           |

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6/4/2025

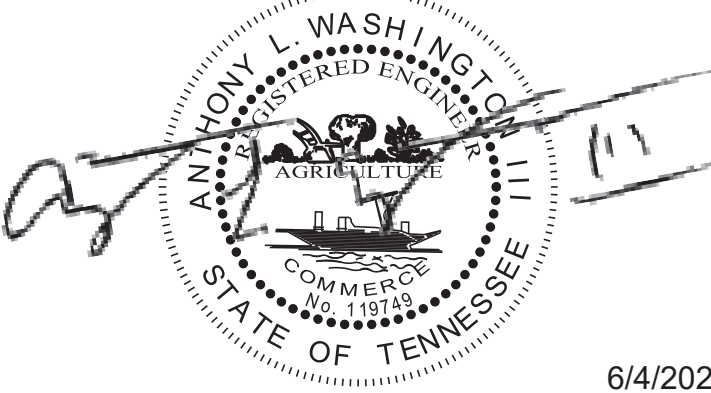
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ROADWAY INDEX,  
STANDARD ROADWAY  
DRAWINGS, AND  
STANDARD TRAFFIC  
DESIGN DRAWINGS

| PROJECT COMMITMENTS |   |   |   |
|---------------------|---|---|---|
| COMMITMENT ID       | SOURCE DIVISON                                | DESCRIPTION   | STA. / LOCATION   |
|                     |   |   |   |
| EDHZ001             | ENVIRONMENTAL DIVISION<br>HAZARDOUS MATERAILS | AN ASBESTOS CONTAINING MATERIAL (ACM) SURVEY WAS COMPLETED ON BRIDGE NO. 39S81350007 SR-201 OVER WEST FORK DOE CREEK LM 6.98 (39-SR201-06.98). NO ASBESTOS WAS DETECTED. PLEASE SEE THE REPORT FOR FURTHER DETAILS AND PHOTOGRAPHS. NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THESE STRUCTURES AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PRIOR TO THE DEMOLITION OR REHABILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (PER TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08.D AND 202.03). | BRIDGE NO. 39S81350007 SR-201 OVER WEST FORK DOE CREEK<br>LM 6.98 |
|                     |   |   |   |
|                     |   |   |   |
|                     |   |   |   |
|                     |   |   |   |
|                     |   |   |   |
|                     |   |   |   |
|                     |   |   |   |

|        |      |               |           |
|--------|------|---------------|-----------|
| TYPE   | YEAR | PROJECT NO.   | SHEET NO. |
| RESURF | 2025 | 39S201-S8-004 | 1B        |
|        |      |               |           |
|        |      |               |           |

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6/4/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PROJECT  
COMMITMENTS




| ESTIMATED ROADWAY QUANTITIES |   |      |                           |
|------------------------------|---|------|---------------------------|
| ITEM NO.                     | DESCRIPTION   | UNIT | QUANTITY<br>39S201-S8-004 |
| (1)                          | 202-03.01 REMOVAL OF ASPHALT PAVEMENT                             | S.Y. | 3606                      |
|                              | 203-06 WATER  | M.G. | 26                        |
|                              | 208-01.05 BROOMING & DEGRASSING SHOULDERS                         | L.M. | 19.6                      |
| (2)                          | 303-02 MINERAL AGGREGATE, TYPE B BASE, GRADING (C OR D)           | TON  | 3411                      |
| (3)(4)                       | 307-01.01 ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING A      | TON  | 1244                      |
| (3)(4)                       | 307-01.08 ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING B-M2   | TON  | 408                       |
| (5)                          | 307-01.10 ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING C-W    | TON  | 11302                     |
| (6)                          | 403-02.01 TRACKLESS TACK COAT                                     | TON  | 29                        |
|                              | 411-12.04 SCORING FOR RUMBLE STRIPE (NON-CONTINUOUS) (4IN WIDTH)  | L.M. | 14.5                      |
| (7)                          | 414-04.03 ASPHALT EMULSION (SCRUB SEAL)                           | TON  | 174                       |
| (7)                          | 414-04.04 MINERAL AGGREGATE (SCRUB SEAL)                          | TON  | 1437                      |
| (8)                          | 415-01.01 COLD PLANING BITUMINOUS PAVEMENT                        | TON  | 154                       |
| (9)                          | 705-02.10 GUARDRAIL TRANSITION 27IN TO 31IN                       | EACH | 26                        |
| (9)                          | 705-04.10 EARTH PAD FOR GUARD RAIL END TREATMENT                  | EACH | 13                        |
| (9)                          | 705-06.11 GR TERMINAL (IN-INLINE) MASH TL3                        | EACH | 2                         |
| (9)                          | 705-06.20 TANGENT ENERGY ABSORBING TERM MASH TL-3                 | EACH | 2                         |
| (9)                          | 705-06.30 GR TERMINAL (ENERGY ABSORBING) MASH TL2                 | EACH | 20                        |
| (9)                          | 706-01 GUARDRAIL REMOVED  | L.F. | 1350                      |
| (9)                          | 706-06.03 RADIUS RAIL   | L.F. | 30                        |
| (10)                         | 712-01 TRAFFIC CONTROL  | LS   | 0.8                       |
| (11)                         | 712-06 SIGNS (CONSTRUCTION)                                       | S.F. | 3072                      |
|                              | 716-01.21 SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR) (1 COLOR) | EACH | 627                       |
| (12)(13)                     | 716-02.05 PLASTIC PAVEMENT MARKING (STOP LINE)                    | L.F. | 110                       |
| (14)                         | 716-05.05 PAINTED PAVEMENT MARKING (STOP LINE)                    | L.F. | 220                       |
| (14)                         | 716-05.20 PAINTED PAVEMENT MARKING (6" LINE)                      | L.M. | 75.6                      |
|                              | 716-08.05 REMOVAL OF PAVEMENT MARKING (STOP LINE)                 | L.F. | 110                       |
|                              | 716-08.20 REMOVAL OF PAVEMENT MARKING (LINE)                      | L.M. | 37.8                      |
| (13)                         | 716-12.02 ENHANCED FLATLINE THERMO PAVMT MRKNG (6IN LINE)         | L.M. | 37.8                      |
|                              | 717-01 MOBILIZATION   | LS   | 0.8                       |

| FOOTNOTES |   |
|-----------|---|
| (1)       | TO BE USED AS DIRECTED BY THE ENGINEER. INCLUDES THE COST OF REMOVING DEBRIS AND SWEEPING SHOULDERS PRIOR TO WORK. SEE SHEET 2C, FINAL PAVEMENT MARKING, NOTE 6 FOR MORE INFORMATION.   |
| (2)       | INCLUDES 2440 TONS FOR BREAKOUT AREAS.  |
| (3)       | ITEM TO BE USED FOR BREAKOUT AREAS.   |
| (4)       | FOR REPLACEMENT OF QUANTITY REMOVED UNDER ITEM 202-03.01  |
| (5)       | INCLUDES 538 TONS FOR PRIVATE DRIVES, BUSINESS ENTRANCES, FIELD ENTRANCES, COUNTY ROADS, INTERSECTIONS, AND EXTRA WIDTH AREAS. INCLUDES 280 TONS FOR SPOT LEVELING.   |
| (6)       | INCLUDES 1 TON FOR PRIVATE DRIVES, BUSINESS ENTRANCES, FIELD ENTRANCES, COUNTY ROADS, INTERSECTIONS, AND EXTRA WIDTH AREAS. THE FINAL SCRUB SEAL SURFACE SHALL BE SWEEPED IMMEDIATELY BEFORE APPLYING TACK COAT. THE SWEEPING IS IN ADDITION TO THE SWEEPING REQUIRED UNDER SPECIAL PROVISION 405SS.  |
| (7)       | BEFORE PLACING SCRUB SEAL THE CONTRACTOR IS REQUIRED TO REMOVE ANY EXISTING THERMOPLASTIC PAVEMENT MARKINGS THAT ARE TO BE COVERED BY SCRUB SEAL, INCLUDING ALL LANE LINES AND SPECIALTY MARKINGS. THE CONTRACTOR SHALL ONLY REMOVE PAVEMENT MARKINGS THAT ARE TO BE COVERED DURING THAT DAY'S CONSTRUCTION ACTIVITIES. THE CONTRACTOR IS TO TAKE EXTRA CARE TO ENSURE THAT THE EXISTING ASPHALT SURFACE IS NOT DAMAGED DURING THERMOPLASTIC REMOVAL. THE ROADWAY MUST BE FREE OF EXCESS DUST OR DEBRIS AS A RESULT OF THERMOPLASTIC REMOVAL BEFORE SCRUB SEAL IS TO BE PLACED. |
| (8)       | INCLUDES TAPER MILLING FROM 0" TO 1.625" FOR A DISTANCE OF 200' AT L.M. 4.60 AND 10.97 ON THE SOUTHERN END OF THE ROADWAY, 10.97 AT THE NORTHERN END OF THE ROADWAY AND AT BOTH BRIDGE ENDS AT STRUCTURES LOCATED AT L.M. 6.97 AND L.M. 13.21.  |
| (9)       | SEE PROPOSED GUARDRAIL (RESURFACING) SHEET 2F.  |
| (10)      | THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.  |
| (11)      | SEE TRAFFIC CONTROL SIGN TABULATION (RESURFACING) ON SHEET 2F.  |
| (12)      | THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.  |
| (13)      | ITEM TO BE USED AS PERMANENT MARKING ONLY.  |
| (14)      | ITEM TO BE USED AS TEMPORARY PAVEMENT MARKING ONLY.   |

| TYPE   | YEAR | PROJECT NO.   | SHEET NO. |
|--------|------|---------------|-----------|
| RESURF | 2025 | 39S201-S8-004 | 2         |
|        |      |               |           |
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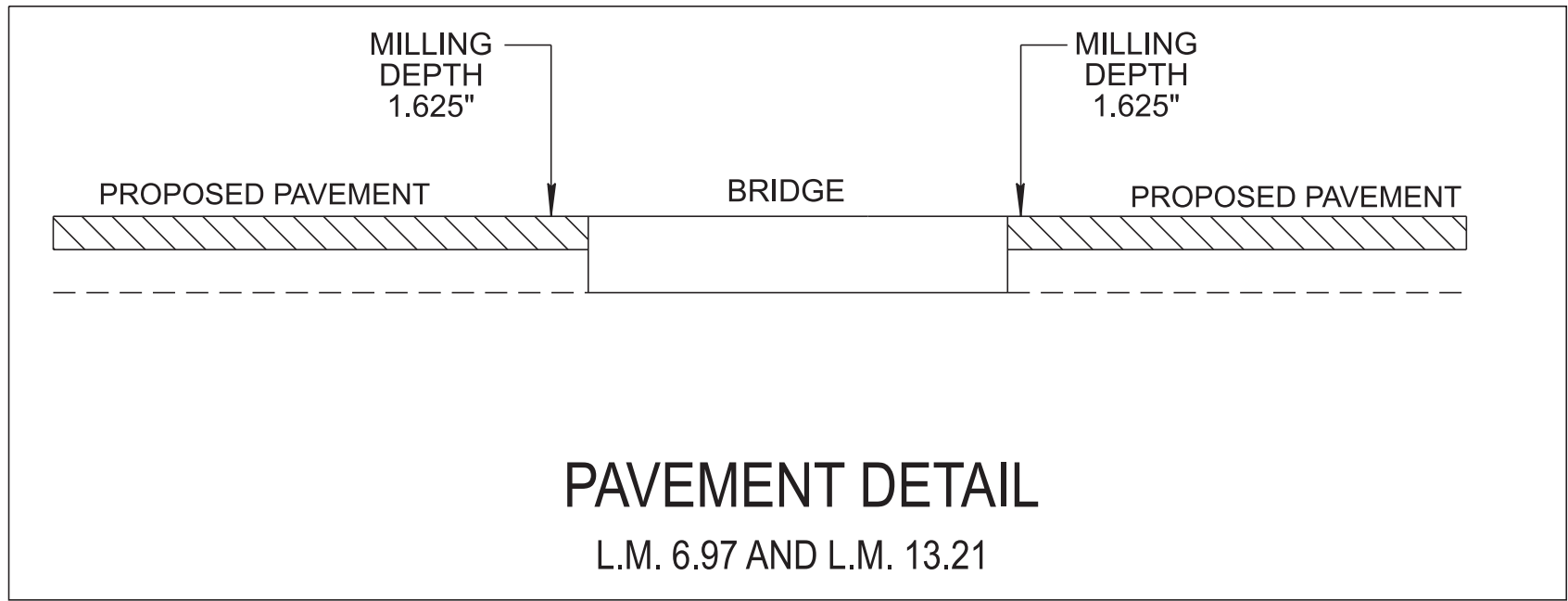
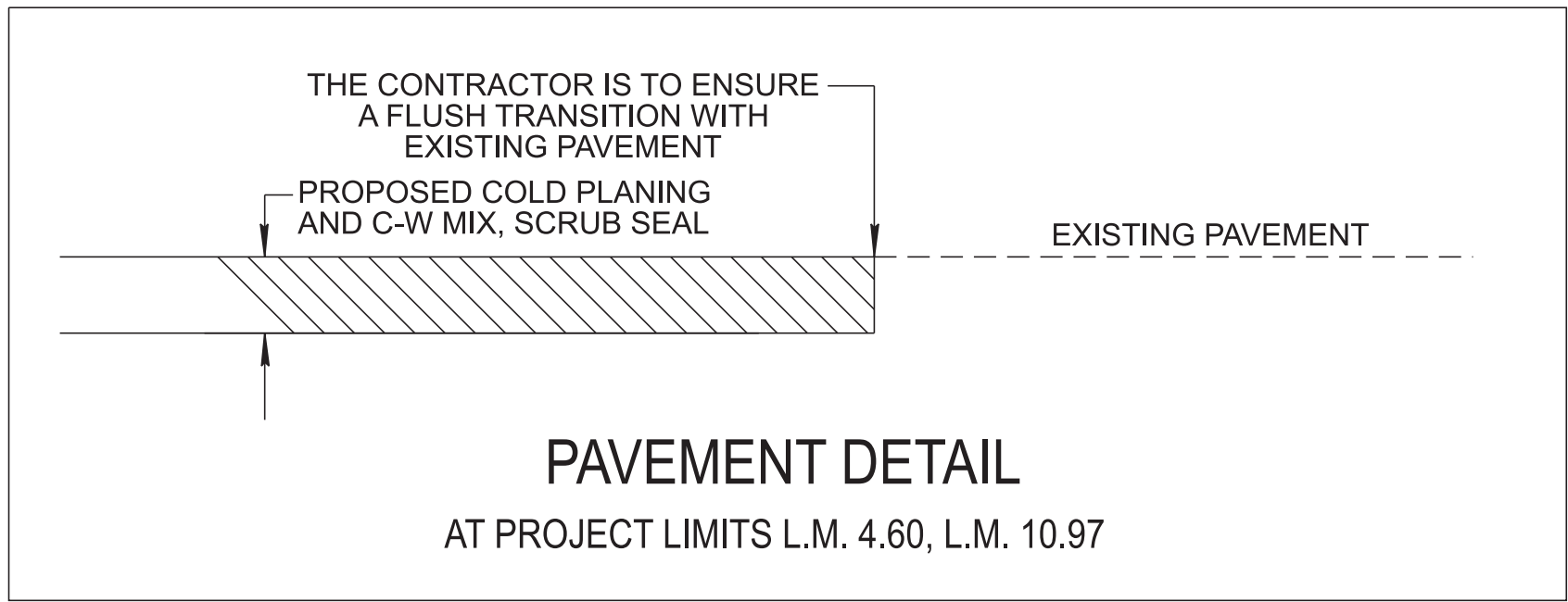
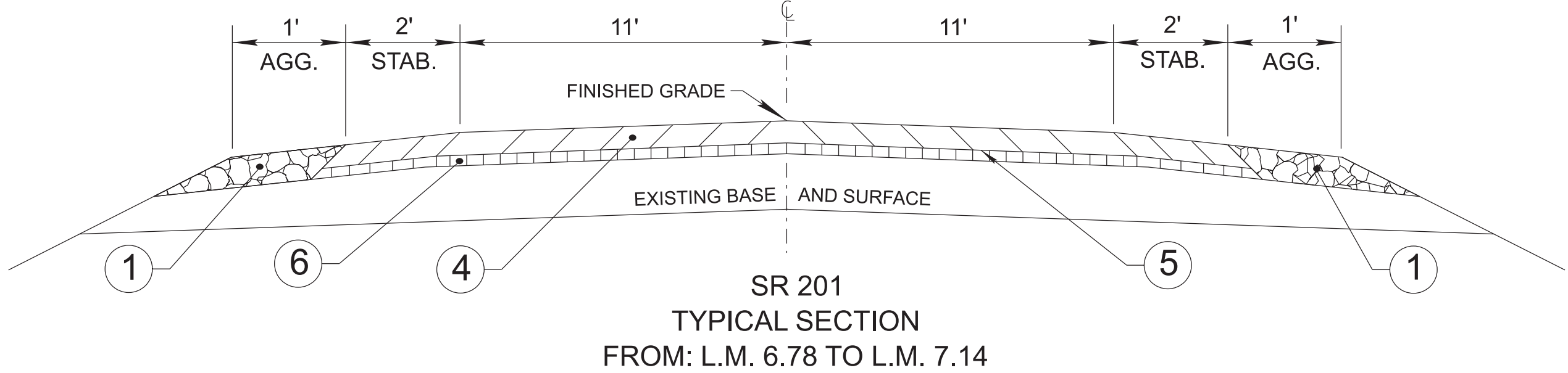
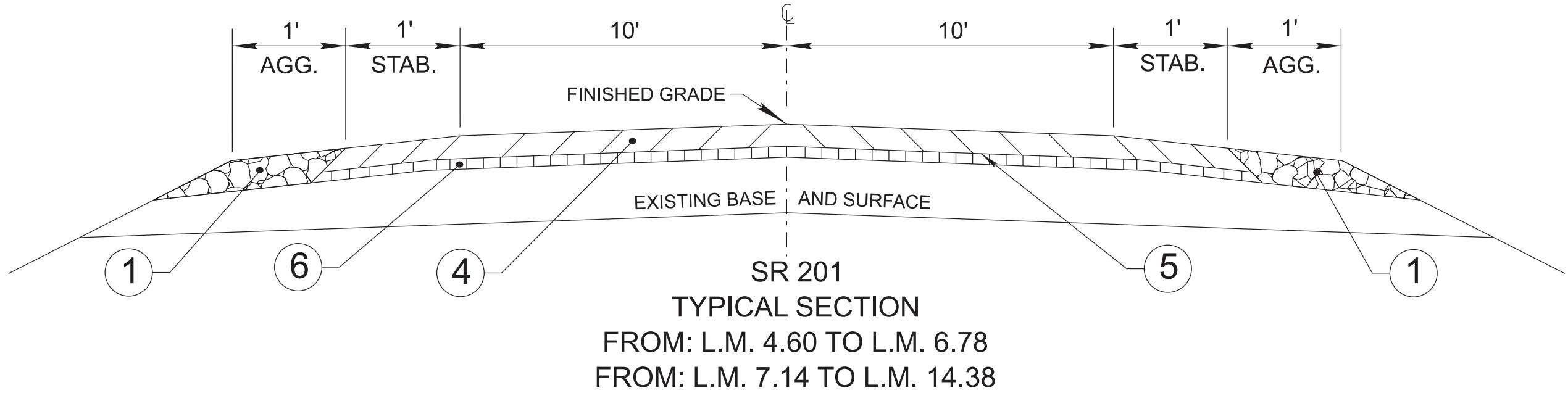


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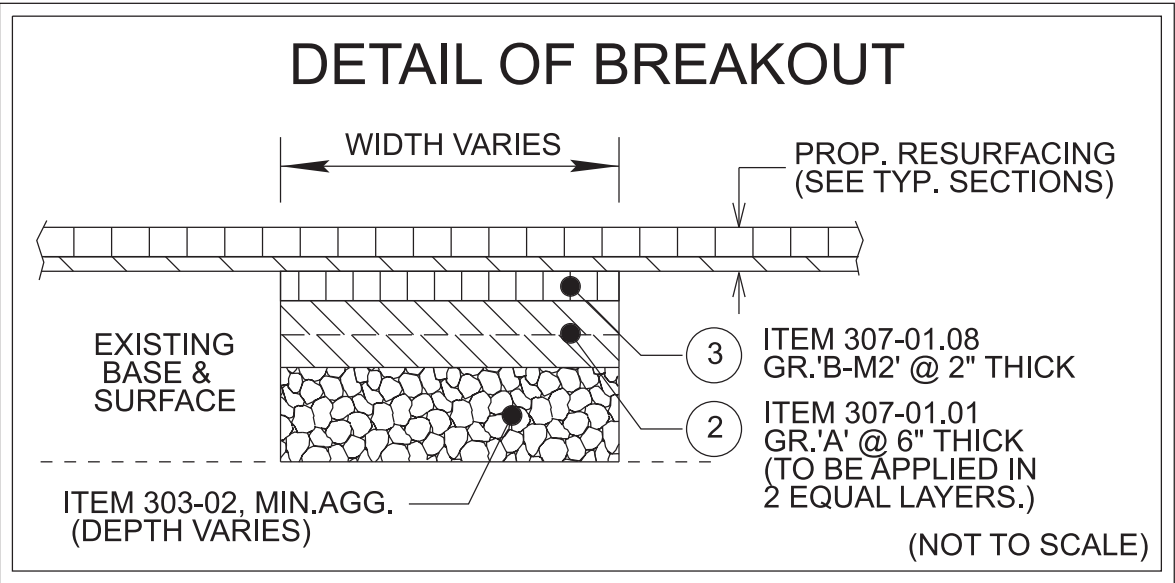
STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ESTIMATED  
ROADWAY  
QUANTITIES

| TYPE   | YEAR | PROJECT NO.   | SHEET NO. |
|--------|------|---------------|-----------|
| RESURF | 2025 | 39S201-S8-004 | 2B        |
|        |      |               |           |
|        |      |               |           |



| PROPOSED PAVEMENT SCHEDULE |  |
|----------------------------|--|
| ①                          | MINERAL AGGREGATE @ 1.5" THICK<br>ITEM 303-02 MINERAL AGGREGATE, TYPE "B" BASE, GRADING "C OR D"   |
| ②                          | BITUMINOUS COURSE (BLACK BASE) @ 6.00"± THICK (APPROX. 690.0 LBS./S.Y.)<br>ITEM 307-01.01 ASPHALT CONCRETE MIX (PG64-22)(BPMB-HM) GRADING "A"<br>(TO BE USED FOR BREAKOUT ONLY)                        |
| ③                          | BITUMINOUS BASE COURSE (BINDER) @ 2.00"± THICK (APPROX. 226.0 LBS./S.Y.)<br>ITEM 307-01.08 ASPHALT CONCRETE MIX (PG64-22)(BPMB-HM) GRADING "B-M2"<br>(TO BE USED FOR BREAKOUT ONLY)                    |
| ④                          | BITUMINOUS COURSE (LEVELING/WEARING) @ 1.50"± THICK (APPROX. 165.00 LBS./S.Y.)<br>ITEM 307-01.10 ASPHALT CONCRETE MIX (PG64-22)(BPMB-HM) GRADING "C-W"   |
| ⑤                          | TACK COAT<br>ITEM 403-02.01 TRACKLESS TACK COAT (TC) (TON) RATE (0.05 GAL./S.Y.)   |
| ⑥                          | SCRUB SEAL<br>ITEM 414-04.03 ASPHALT EMULSION (SCRUB SEAL)<br>ASPHALT EMULSION (AT 0.25 - 0.35 GAL./S.Y.)<br>ITEM 414-04.04 MINERAL AGGREGATE (SCRUB SEAL)<br>MINERAL AGGREGATE (AT 18 - 25 LBS./S.Y.) |



| BRIDGE NOTES  |                   |                        |               |  |
|---------------|-------------------|------------------------|---------------|--|
| BRIDGE NUMBER | LOCATION LOG MILE | CROSSES OVER/UNDER     | BRIDGE LENGTH | BRIDGE DECK RECOMMENDATIONS  |
| 39S81350005   | 5.710             | Branch                 | 26'           | PAVE WITH PLANS MIX/TREATMENT TYPE   |
| 39S81350007   | 6.970             | West Prong Doe Creek   | 99'           | PROVIDE DECK REPAIR ITEM- SEAL WITH TYPE 1 THIN EPOXY OVERLAY (BRIDGE SHEETS INCLUDED) |
| 39S81350009   | 8.330             | Middle Prong Doe Creek | 21.7'         | REPLACE BRIDGE JOINTS (BRIDGE SHEETS INCLUDED)   |
| 39016980001   | 13.210            | Flat Creek             | 125'          | PAVE WITH PLANS MIX/TREATMENT TYPE   |
|               |                   |                        |               | LEAVE AS IS  |

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6/4/2025

NOT TO SCALE

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TYPICAL  
SECTIONS AND  
PAVEMENT  
SCHEDULE



GENERAL NOTES

GRADING

- (1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.
- (3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

GUARDRAIL

- (1) THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPES UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.
- (3) IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE “A” LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING TEMPORARY BARRICADES OR DRUMS WITH TYPE “A” LIGHTS TO DELINEATE GUARDRAIL END AND A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL END TERMINAL.

MISCELLANEOUS

- (2) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES AND POSTS WHERE AND AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN PRICE BID FOR OTHER CONSTRUCTION ITEMS.
- (3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

- (2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6” LINE), L.M.

FINAL PAVEMENT MARKING

- (6) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:
- a. BROOMING & DE-GRASSING SHOULDERS SHALL INCLUDE CLIPPING OF MATERIAL INTERFERING WITH PROPER DRAINAGE OF ROADWAY (INCLUDING PAVED AND GRAVEL SHOULDERS), AS DIRECTED BY THE TDOT PROJECT ENGINEER.

b. ALL MATERIAL FROM CLIPPING, BROOMING AND DE-GRASSING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE TDOT PROJECT ENGINEER.

c. ALL COSTS ASSOCIATED WITH PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.

d. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

- (8) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6” ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY’S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

PAVEMENT

PAVING

- (1) THE CONTRACTOR SHALL BE REQUIRED TO PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

- (4) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (5) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.
- (6) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.
- (9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

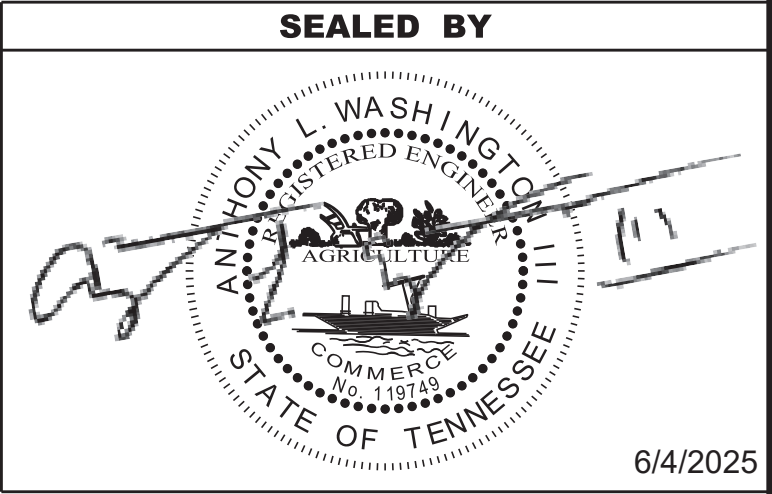
SIGNING

- (12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

- (1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.
- (2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.
- (3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.
- (4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.
- (5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.
- (6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT’S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT’S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER’S APPROVAL TO USE THEM.
- (7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- (9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

GENERAL  
NOTES



SPECIAL NOTES

RESURFACING

- (1) SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.
- (4) THE OUTSIDE SHOULDER WILL BE PAVED CONCURRENTLY WITH THE TRAFFIC LANE.
- (6) THE BITUMINOUS MATERIAL DESIGNATED TO RESTORE THE COLD PLANING AREA WILL BE PLACED WITHIN 96 HOURS OF THE COMMENCEMENT OF COLD PLANING OPERATIONS. COLD PLANING OPERATIONS WILL BE LIMITED TO AN AREA EQUAL TO THAT WHICH CAN BE COVERED WITH BITUMINOUS MATERIAL WITHIN THE TIME LIMITS SPECIFIED, EVEN IF COLD PLANING OPERATIONS MUST BE SUSPENDED UNTIL PAVING CATCHES UP.

PAVEMENT MARKING

- (1) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.20
- (2) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-12.02.

SIGNS

- (1) IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION, THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

MISCELLANEOUS


- (1) ITEM 303-02 TO BE PLACED BEFORE PLACING SURFACING MATERIAL.
- (2) THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ANY SIGNS AND MAILBOXES DURING THE OPERATION. ANY SIGNS OR MAILBOXES DAMAGED AS A RESULT OF THE OPERATIONS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

SCRUB SEAL

- (1) BEFORE PLACING SCRUB SEAL, ITEM NOS. 414-04.03 AND 414-04.04, THE CONTRACTOR IS REQUIRED TO REMOVE ANY EXISTING THERMOPLASTIC PAVEMENT MARKINGS THAT ARE TO BE COVERED BY SCRUB SEAL, INCLUDING ALL LANE LINES AND SPECIALTY MARKINGS. THE CONTRACTOR SHALL ONLY REMOVE PAVEMENT MARKINGS THAT ARE TO BE COVERED DURING THAT DAY'S CONSTRUCTION ACTIVITIES. THE CONTRACTOR IS TO TAKE EXTRA CARE TO ENSURE THAT THE EXISTING ASPHALT SURFACE IS NOT DAMAGED DURING THERMOPLASTIC REMOVAL. THE ROADWAY MUST BE FREE OF EXCESS DUST OR DEBRIS AS A RESULT OF THERMOPLASTIC REMOVAL BEFORE SCRUB SEAL IS TO BE PLACED.

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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

SPECIAL  
NOTES



ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

- (4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.
- (9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

- (11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).
- (12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

- (15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ENGINEERING PRODUCTION SUPPORT DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

- (1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

- (2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.
- (3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.
- (4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

SCOPE OF WORK

- (6) THIS PROJECT INCLUDES SCRUB SEAL & 307 CW, PAVEMENT MARKINGS, GUARDRAIL, TEMPORARY TRAFFIC CONTROL, AND BROOMING AND DEGRASSING SHOULDERS.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

- (1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL


- (6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.
- (8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

- (29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.
- (30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.
- (31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.
- (32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.
- (33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.
- (34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.
- (35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.
- (36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.
- (37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.
- (38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.
- (39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.
- (40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL  
NOTES




ENVIRONMENTAL NOTES (CONT.)

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

- (44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.
- (45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.
- (46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.
- (47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.
- (48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.
- (49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.
- (50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.
- (51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.
- (52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.
- (53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL  
NOTES



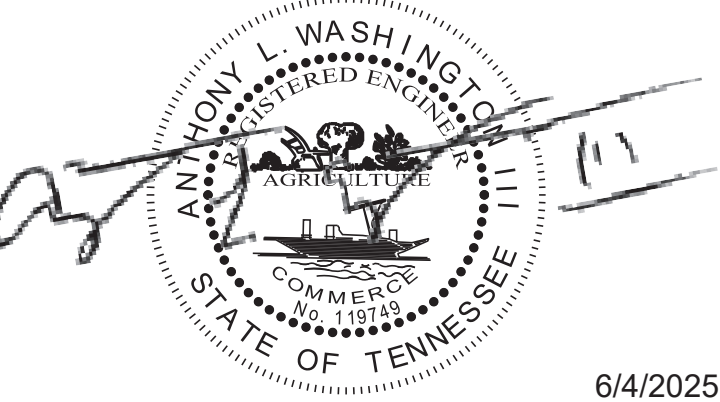
| TRAFFIC CONTROL SIGN TABULATION (RESURFACING)   |                                  |                                  |      |                             |                            |
|---|----------------------------------|----------------------------------|------|-----------------------------|----------------------------|
| M.U.T.C.D.<br>SIGN<br>NO.   | LEGEND \ DESCRIPTION             | SIZE<br>IN INCHES<br>L    x    W | S.F. | TOTAL<br>NUMBER<br>REQUIRED | ITEM NO.<br>712-06<br>S.F. |
| G20-1   | ROAD WORK NEXT 5 MILES           | 48"   x   24"                    | 8    | 1                           | 8                          |
| G20-1   | ROAD WORK NEXT 7 MILES           | 48"   x   24"                    | 8    | 2                           | 16                         |
| G20-2   | END ROAD WORK                    | 48"   x   24"                    | 8    | 31                          | 248                        |
| W8-7  | LOOSE GRAVEL                     | 48"   x   48"                    | 16   | 44                          | 704                        |
| W8-11   | UNEVEN LANES                     | 48"   x   48"                    | 16   | 44                          | 704                        |
| W20-1   | ROAD WORK 1 MILE                 | 48"   x   48"                    | 16   | 3                           | 48                         |
| W20-1   | ROAD WORK 1/2 MILE               | 48"   x   48"                    | 16   | 3                           | 48                         |
| W20-1   | ROAD WORK 1000 FT                | 48"   x   48"                    | 16   | 3                           | 48                         |
| W20-1   | ROAD WORK AHEAD                  | 48"   x   48"                    | 16   | 28                          | 448                        |
| W20-4   | ONE LANE ROAD 1000 FT - PORTABLE | 48"   x   48"                    | 16   | 2                           | 32                         |
| W20-7   | FLAGGER (SYMBOL) - PORTABLE      | 48"   x   48"                    | 16   | 2                           | 32                         |
| W21-2   | FRESH OIL - PORTABLE             | 48"   x   48"                    | 16   | 2                           | 32                         |
| W21-15  | SHOULDER WORK                    | 48"   x   48"                    | 16   | 44                          | 704                        |
| THIS CONSTRUCTION SIGNING IS TO BE AS A MINIMUM. OTHER SIGNS AS DIRECTED BY THE ENGINEER MAY BE REQUIRED DURING DIFFERENT PHASES. |                                  |                                  |      | TOTAL                       | 3072                       |

| UTILITY ADJUSTMENTS (RESURFACING) |             |        |
|-----------------------------------|-------------|--------|
| TYPE                              | OWNER       | NUMBER |
| WATER VALVE                       | SCOTTS HILL | 1      |

| PROPOSED GUARDRAIL (RESURFACING) |    |          |                              |   |                     |                      |                     |                                  |                                  |
|----------------------------------|----|----------|------------------------------|---|---------------------|----------------------|---------------------|----------------------------------|----------------------------------|
| SIDE                             |    | LOG MILE | GUARDRAIL                    |   |                     |                      | TERMINAL ANCHOR     |                                  |                                  |
|                                  |    |          | TRANSITION<br>27 IN TO 31 IN | EARTH PAD<br>FOR GUARD<br>RAIL END<br>TREATMENT | RADIUS<br>RAIL      | GUARDRAIL<br>REMOVED | IN-LINE<br>MASH TL3 | TYPE 38<br>MASH TL3<br>(26.896') | TYPE 21<br>MASH TL2<br>(21.875') |
| LT                               | RT |          | 705-02.10<br>(EACH)          | 705-04.10<br>EACH                               | 706-06.03<br>(L.F.) | 706-01<br>(L.F.)     | 705-06.11<br>(EACH) | 705-06.20<br>(EACH)              | 705-06.30<br>(EACH)              |
|                                  | X  | 5.588    | 1                            |   |                     | 50                   |                     |                                  |                                  |
| X                                |    | 5.704    | 1                            |   |                     | 50                   |                     |                                  |                                  |
|                                  | X  | 5.706    | 1                            |   |                     | 50                   |                     |                                  |                                  |
| X                                |    | 5.718    | 1                            |   |                     | 50                   |                     |                                  |                                  |
|                                  | X  | 5.719    | 1                            |   |                     | 50                   |                     |                                  |                                  |
| X                                |    | 6.948    | 1                            |   |                     | 50                   |                     | 1                                |                                  |
|                                  | X  | 7.007    | 1                            |   |                     | 50                   |                     | 1                                |                                  |
| X                                |    | 7.676    | 1                            | 1   |                     | 50                   |                     |                                  | 1                                |
|                                  | X  | 7.680    | 1                            | 1   |                     | 50                   |                     |                                  | 1                                |
| X                                |    | 7.709    | 1                            | 1   |                     | 50                   |                     |                                  | 1                                |
|                                  | X  | 7.714    | 1                            | 1   |                     | 50                   |                     |                                  | 1                                |
| X                                |    | 8.071    | 1                            | 1   |                     | 50                   |                     |                                  | 1                                |
|                                  | X  | 8.097    | 1                            | 1   |                     | 50                   |                     |                                  | 1                                |
| X                                |    | 8.144    | 1                            | 1   | 15                  | 50                   | 1                   |                                  | 1                                |
|                                  | X  | 8.153    | 1                            | 1   |                     | 50                   |                     |                                  | 1                                |
| X                                |    | 8.312    | 1                            |   |                     | 50                   |                     |                                  | 1                                |
|                                  | X  | 8.313    | 1                            |   |                     | 50                   |                     |                                  | 1                                |
| X                                |    | 8.345    | 1                            |   |                     | 50                   |                     |                                  | 1                                |
|                                  | X  | 8.347    | 1                            |   |                     | 50                   |                     |                                  | 1                                |
|                                  | X  | 9.259    | 1                            | 1   |                     | 50                   |                     |                                  | 1                                |
| X                                |    | 9.273    | 1                            | 1   |                     | 50                   |                     |                                  | 1                                |
| X                                |    | 9.301    | 1                            | 1   |                     | 50                   |                     |                                  | 1                                |
|                                  | X  | 9.302    | 1                            | 1   |                     | 50                   |                     |                                  | 1                                |
|                                  | X  | 13.204   | 1                            | 1   |                     | 50                   |                     |                                  | 1                                |
| X                                |    | 13.208   |                              |   | 15                  | 50                   | 1                   |                                  | 1                                |
|                                  | X  | 13.241   | 1                            |   |                     | 50                   |                     |                                  | 1                                |
| X                                |    | 13.245   | 1                            |   |                     | 50                   |                     |                                  | 1                                |
| TOTALS                           |    |          | 26                           | 13  | 30                  | 1350                 | 2                   | 2                                | 20                               |

| TYPE   | YEAR | PROJECT NO.   | SHEET NO. |
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| RESURF | 2025 | 39S201-S8-004 | 2F        |
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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

TABULATED  
QUANTITIES

UTILITY NOTES

UTILITY

- (2)

UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.
- (3)

THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.
- (4)

PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.
- (5)

THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

**ELECTRIC:**  
**STEMC**  
1009 E. MAIN STREET  
BROWNSVILLE, TN 38012  
CONTACT: JOSHUA KENNEDY  
OFFICE PHONE: 731 585 0531  
CELL PHONE:  
Email: JKENNEDY@STEMC.COM

**COMMUNICATIONS:**  
**AT&T**  
315 E. COLLEGE STREET  
JACKSON, TN 38301  
CONTACT: DANIEL R. POTTS  
OFFICE PHONE: 901 488 2359  
CELL PHONE:  
Email: DP7607@ATT.COM


**GAS/WATER/SEWER:**  
**HENDERSON UTILITY DEPARTMENT**  
121 CROOK AVE  
HENDERSON, TN 38340  
CONTACT: DARRYL GREEN  
OFFICE PHONE: 731 983 5020  
CELL PHONE: 731 608 3738  
Email: DGREEN@HENDERSONTN.GOV

**CABLE:**  
**CHARTER COMMUNICATIONS**  
24 CIRCLE DRIVE  
McKENZIE, TN 38201  
CONTACT: KEITH CHESSER  
OFFICE PHONE: 731 352 1146  
CELL PHONE: 731 621 9552  
Email: KEITH.CHESSER@CHARTER.COM

**ELECTRIC:**  
**PICKWICK ELECTRIC CO.**  
672 HWY 142  
SELMER, TN 38375  
CONTACT: JOHN HUGHES  
OFFICE PHONE: 731 645 3411  
CELL PHONE: 731 434 0619  
Email: JHUGHES@PICKWICK-ELECTRIC.COM

| TYPE   | YEAR | PROJECT NO.   | SHEET NO. |
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| RESURF | 2025 | 39S201-S8-004 | 3         |
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STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

UTILITY NOTES  
AND  
UTILITY OWNERS



PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:

a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.

2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.

a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.
- IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.
3. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

a. THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.

b. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.

c. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.

d. THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.

FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

4. FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.

SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.

IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:

1. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 3/4 INCH AND NOT EXCEEDING 2 INCHES.

a. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

2. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:

a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

3. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:

a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

(1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

(2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

b. ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:


1. WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

2. WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

| TYPE   | YEAR | PROJECT NO.   | SHEET NO. |
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| RESURF | 2025 | 39S201-S8-004 | 4         |
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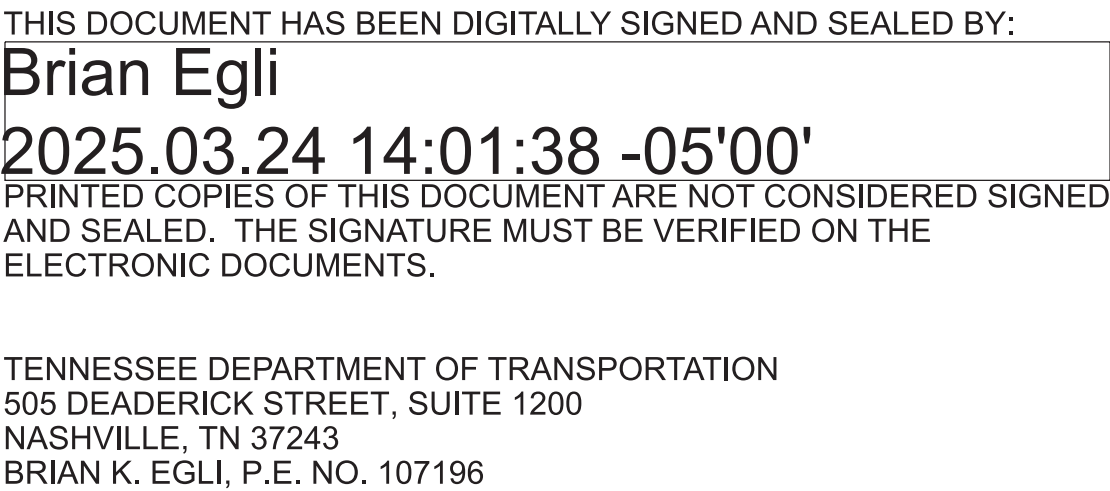
6/4/2025

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE  
DROP-OFF NOTES  
FOR  
TRAFFIC CONTROL

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| SHEET NAME      | SHEET NO.      |
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| BRIDGE PLANS    | B1 THRU B6     |

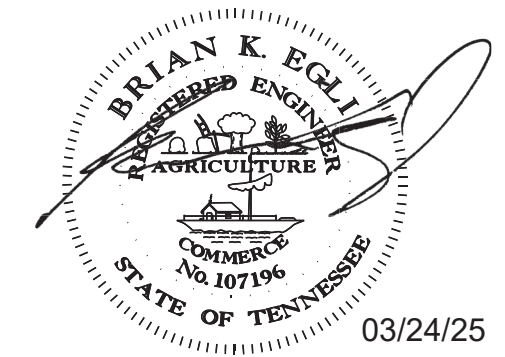
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| <b>SIGNATURE</b><br><b>SHEET</b>                                 |               |                  |



PIN 133165.00

[illegible]

| INDEX OF REFERENCE DRAWINGS | DWG. NO. |
|-----------------------------|----------|
| LAYOUT                      | M-442-96 |
| SUPERSTRUCTURE              | M-442-99 |



DEPARTMENT OF TRANSPORTATION  
INDEX OF DRAWINGS  
39-SR201-6.97  
OVER  
WEST PRONG DOE CREEK  
BR. NO. 39S81350007  
HENDERSON COUNTY  
2025

DESIGNED BY \_\_\_\_\_ DATE \_\_\_\_\_  
DRAWN BY Z.HAYNES DATE 10/24  
SUPERVISED BY K. MARTINKO DATE 10/24  
CHECKED BY \_\_\_\_\_ DATE \_\_\_\_\_



[illegible]

# EXPANSION JOINT REPAIR NOTES

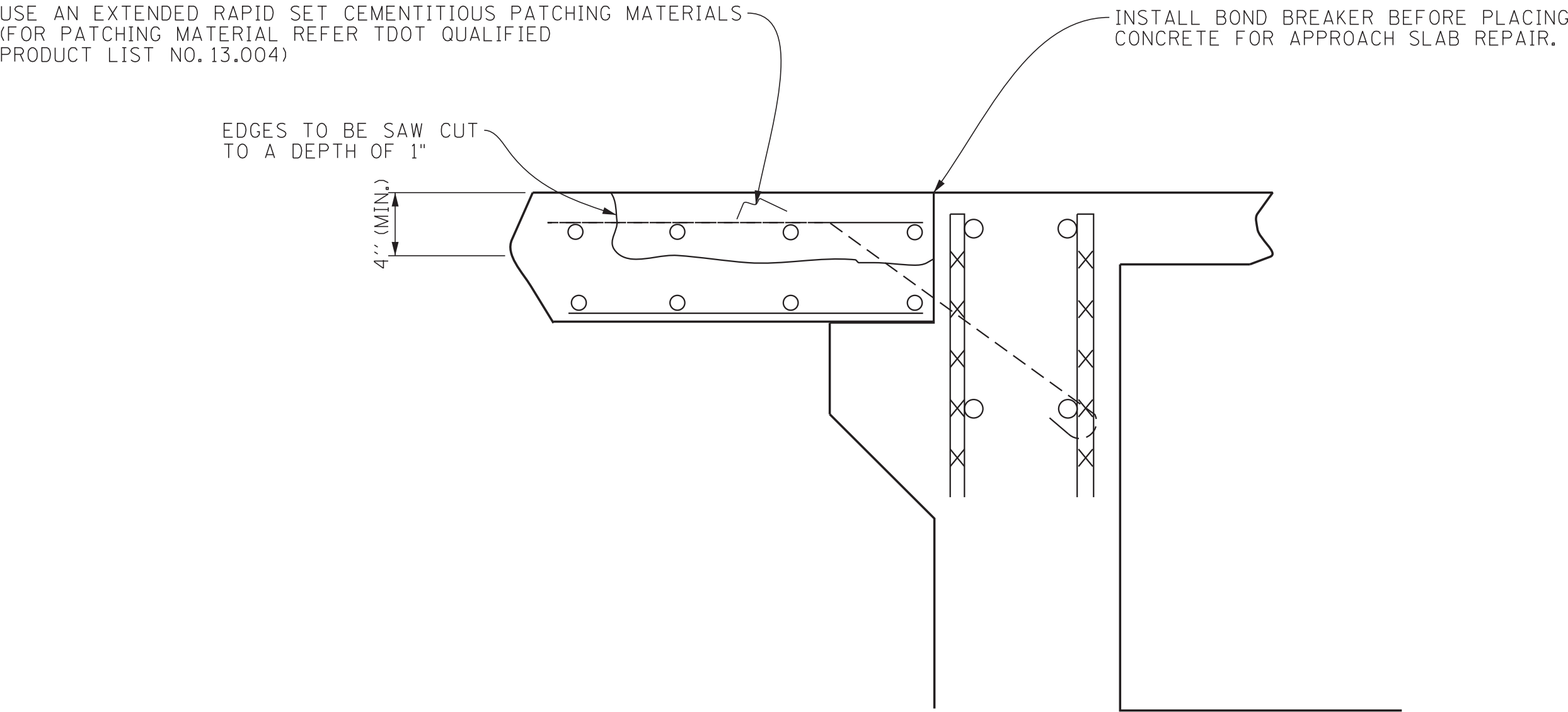
BRIDGE TABULATION,  
ESTIMATED QUANTITIES, AND  
EXPANSION JOINT REPAIR  
NOTES  
39-SR201-6.97  
OVER  
WEST PRONG DOE CREEK  
BR. NO. 39S81350007  
HENDERSON COUNTY  
2025

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| PROJECT NO.   |      | YEAR | SHEET NO.         |
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| 39S201-M3-003 |      | 2025 | B3                |
| REVISIONS     |      |      |                   |
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CONCRETE APPROACH PAVEMENT REPAIR DETAILS (STRUCTUAL REPAIR)

NOTE: REMOVE CONCRETE TO A DEPTH OF ¾" MINIMUM BELOW EXISTING REINFORCING STEEL OR TO SOUND CONCRETE. CONCRETE REMOVAL SHOULD BE TO A MINIMUM DEPTH OF 4". DEPTH FOR CONCRETE REMOVAL SHOULD NOT BE MORE THAN 6".

NOTE: COST OF SAW CUTTING, CONCRETE REMOVAL, COMPLETELY CLEANING EXPOSED REINFORCING STEEL, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO COMPLETE THE REPAIRS AS SHOWN SHALL BE INCLUDED IN ITEM NO. 604-10.53 CONCRETE REPAIR (S.Y.).

NOTE: ITEM NO. 604-10.53, CONCRETE REPAIR (PARTIAL DEPTH OF APPROACH PAVEMENT.) THIS MAY BE INCREASED, DECREASED OR ELIMINATED AS DIRECTED BY THE ENGINEER.

POWER DRIVEN HAND TOOLS:

POWER DRIVEN HAND TOOLS USED FOR THE REMOVAL OF UNSOUND CONCRETE IN MAKING PARTIAL DEPTH REPAIRS ARE SUBJECT TO THE FOLLOWING RESTRICTIONS:

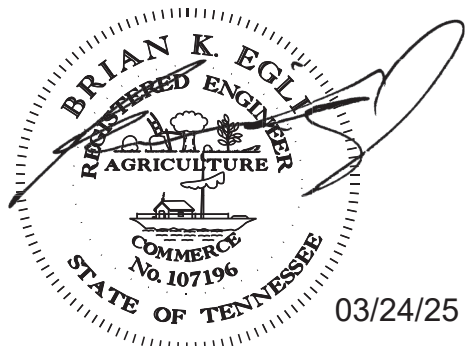
(1) PARTIAL DEPTH REPAIRS; PNEUMATIC HAMMERS HEAVIER THAN NOMINAL 60 LB. CLASS SHALL NOT BE USED. TRAFFIC CONTROL SHALL BE SET UP DURING PARTIAL DEPTH REPAIRS OVER TRAFFIC.

(2) CHIPPING HAMMERS OF THE 15 LB. CLASS SHALL BE USED TO REMOVE CONCRETE FROM BENEATH ANY REINFORCING STEEL.

BRIDGE DECK REPAIR MATERIAL:

REPAIRS SHALL USE AN EXTENDED NON-MAGNESIUM PHOSPHATE PRODUCT FROM THE TDOT QUALIFIED PRODUCTS LIST: 13.004-RAPID SET CEMENTITIOUS PATCHING MATERIALS. MATERIAL SHALL BE APPLIED ACCORDING TO MANUFACTURER'S SPECIFICATIONS.

MIX MUST MEET 3000 PSI BEFORE OPENING TO TRAFFIC.



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
APPROACH SLAB REPAIR  
DETAIL NOTES  
39-SR201-6.97  
OVER  
WEST PRONG DOE CREEK  
BR. NO. 39S81350007  
HENDERSON COUNTY  
2025

DESIGNED BY \_\_\_\_\_ DATE \_\_\_\_\_  
DRAWN BY Z.HAYNES DATE 10/24  
SUPERVISED BY K. MARTINKO DATE 10/24  
CHECKED BY \_\_\_\_\_ DATE \_\_\_\_\_



[illegible]

TYPE 1 THIN EPOXY OVERLAY SYSTEM - USE DECK PRETREATMENT/PRIMER PER MANUFACTURER'S RECOMMENDATION, AND 2 LIFTS OF AN EPOXY-URETHANE COPOLYMER AND AGGREGATE. TYPE 1 OVERLAY SHALL BE APPLIED MECHANICALLY USING METERED EQUIPMENT; HAND MIXING OF MATERIAL IS NOT PERMITTED.

APPLICATION EQUIPMENT SHOULD :

B) USE AN APPLICATION MACHINE THAT FEATURES POSITIVE DISPLACEMENT VOLUMETRIC METERING PUMPS CONTROLLED BY A HYDRAULIC POWER UNIT.

D) CHECK MIXING RATIO AT THE PUMP OUTLETS AS WELL AS CYCLE COUNTING CAPABILITIES TO MONITOR OUTPUT ON STANDARD FEATURES.

F) MAXIMIZE MATERIAL WORKING TIME BY MIXING IT IMMEDIATELY BEFORE DISPENSING.

| SIEVE SIZE | % PASSING |
|------------|-----------|
|------------|-----------|

NO. 6 95-100

NO. 10 10-35

NO. 20 0-3

THE CONCRETE DECK SURFACE SHALL BE CLEANED BY SHOTBLASTING TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPING, OR ANY OTHER POTENTIAL DETRIMENTAL MATERIAL SUCH AS CURING COMPOUND AND LAITANCES, WHICH THE MANUFACTURER AND ENGINEER'S OPINION WOULD PREVENT PROPER BONDING AND CURING OF THE MATERIAL. IN AREAS WHERE SHOTBLASTING EQUIPMENT CAN NOT REACH (I.E., ALONG CURBS AND BRIDGE RAILS) SANDBLASTING IS PERMITTED TO AN EXTENT TO THE ENGINEER'S AND MANUFACTURER'S APPROVAL. IMMEDIATELY BEFORE APPLICATION, ALL PREPARED SURFACES SHALL BE CLEANED WITH COMPRESSED AIR OR VACUUMED TO REMOVE DUST AND DEBRIS.

BLUSHING (A WAXY SURFACE COATING ON THE EPOXY) IS CAUSED BY THE REACTION OF MOISTURE WITH THE HARDENING AGENT. BLUSHING CREATES A SURFACE THAT MAKES FUTURE LAYERS DIFFICULT TO ADHERE. LIFTS THAT SHOW SIGNS OF BLUSHING SHALL BE REMOVED AND REPLACED PRIOR TO APPLICATION OF THE NEXT. THE COST TO REMOVE AND REPLACE THESE AREAS SHALL BE AT THE CONTRACTOR'S EXPENSE.

TRAFFIC, OTHER THAN APPLICATION EQUIPMENT, SHALL NOT BE ALLOWED ON ANY PORTION OF THE DECK THAT HAS BEEN SHOTBLASTED OR WHERE PART OF THE APPLICATION HAS BEEN PLACED.

SEE MANUFACTURER'S RECOMMENDATIONS FOR REQUIRED AMBIENT AND SURFACE TEMPERATURES AND HUMIDITY LIMITS FOR APPLICATION.

ALL COSTS FOR AGGREGATE, EPOXY FOR MINIMUM OF TWO LIFTS, SURFACE PREPARATION, LABOR AND ANY OTHER MISCELLANEOUS MATERIALS REQUIRED TO PLACE THIN OVERLAY SHALL BE INCLUDED IN ITEM NO. 617-04.01, TYPE 1 THIN EPOXY OVERLAY (EPOXY URETHANE), SY.

THICKNESS VERIFICATION: THE PROJECT ENGINEER SHALL BE NOTIFIED OF THE NUMBER OF GALLONS USED ON THE PROJECT WITH NOTARIZED QUANTITY STATEMENTS FROM THE CONTRACTOR AND THE MANUFACTURER. THE CONTRACTOR SHALL VERIFY TO TDOT THAT THE OVERLAY IS AN AVERAGE OF AT LEAST 3/8 INCH THICK AT THREE RANDOM LOCATIONS AGREED UPON BY THE PROJECT ENGINEER AND THE MATERIAL MANUFACTURER REPRESENTATIVE. IF 3/8 INCH AVERAGE IS NOT ACHIEVED, A RETEST SHALL BE PERFORMED IN ADJOINING AREAS, SHALL BE RE-COATED AS DESCRIBED ABOVE BY THE CONTRACTOR AND RE-VERIFIED AT NO ADDITIONAL COST TO TDOT. THIS VERIFICATION MAY CONSIST OF CORES, HOLES, ETC., BUT IN ALL CASES, ANY DESTRUCTIVELY TESTED AREAS SHALL BE REPAIRED BY THE CONTRACTOR BEFORE FINAL ACCEPTANCE BY THE PROJECT ENGINEER.

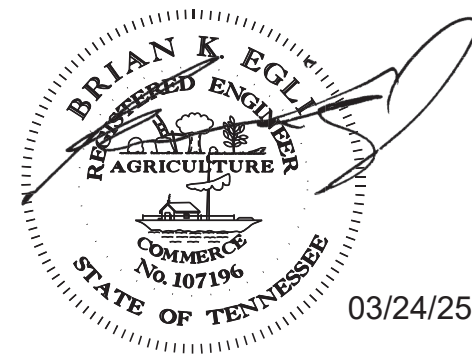
DESIGNED BY \_\_\_\_\_ DATE \_\_\_\_\_  
 DRAWN BY Z.HAYNES DATE 10/24  
 SUPERVISED BY K. MARTINKO DATE 10/24  
 CHECKED BY \_\_\_\_\_ DATE \_\_\_\_\_

\*\* SPECIAL NOTE:

THE CONTRACTOR IS TO PREVENT THE TRACKING OF TACKCOAT AND CONSTRUCTION DEBRIS ACROSS THE BRIDGE DECK PRIOR TO APPLICATION OF THE THIN EPOXY OVERLAY.

MILLING THE BRIDGE DECK WILL NOT BE AN OPTION FOR TACKCOAT OR DEBRIS REMOVAL.

REMOVAL SHALL BE AT THE CONTRACTOR'S EXPENSE.



STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
TYPE I THIN EPOXY  
OVERLAY NOTES  
39-SR201-6.97  
OVER  
WEST PRONG DOE CREEK  
BR. NO. 39S81350007  
HENDERSON COUNTY  
2025

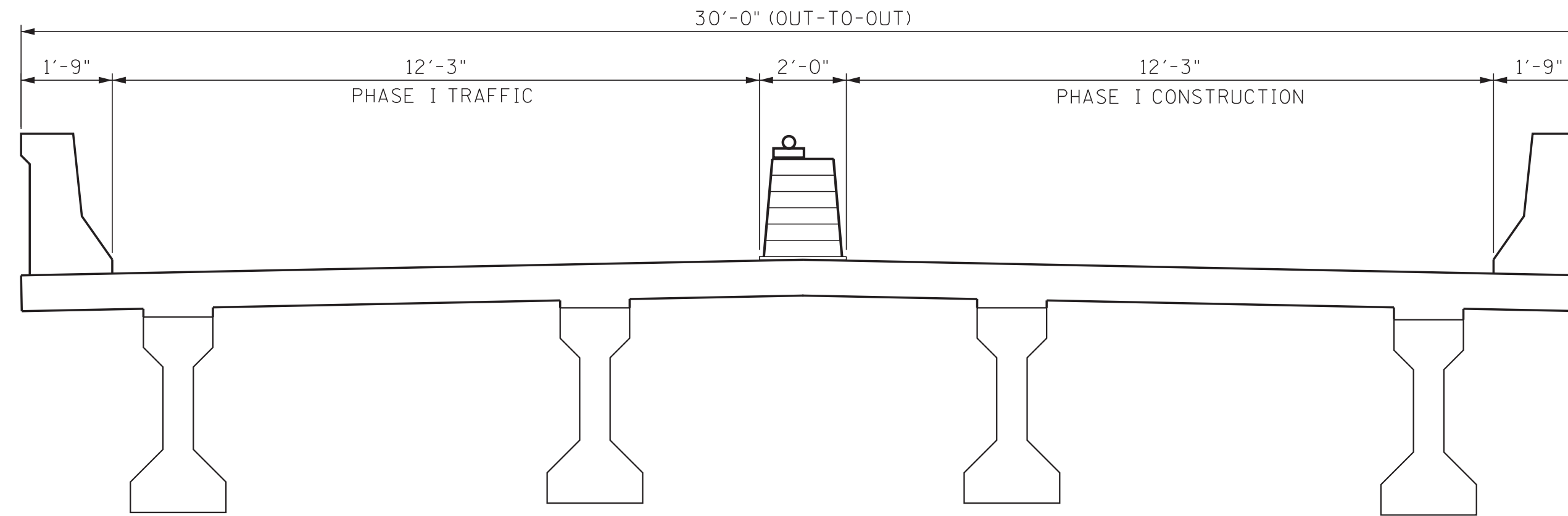


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B5

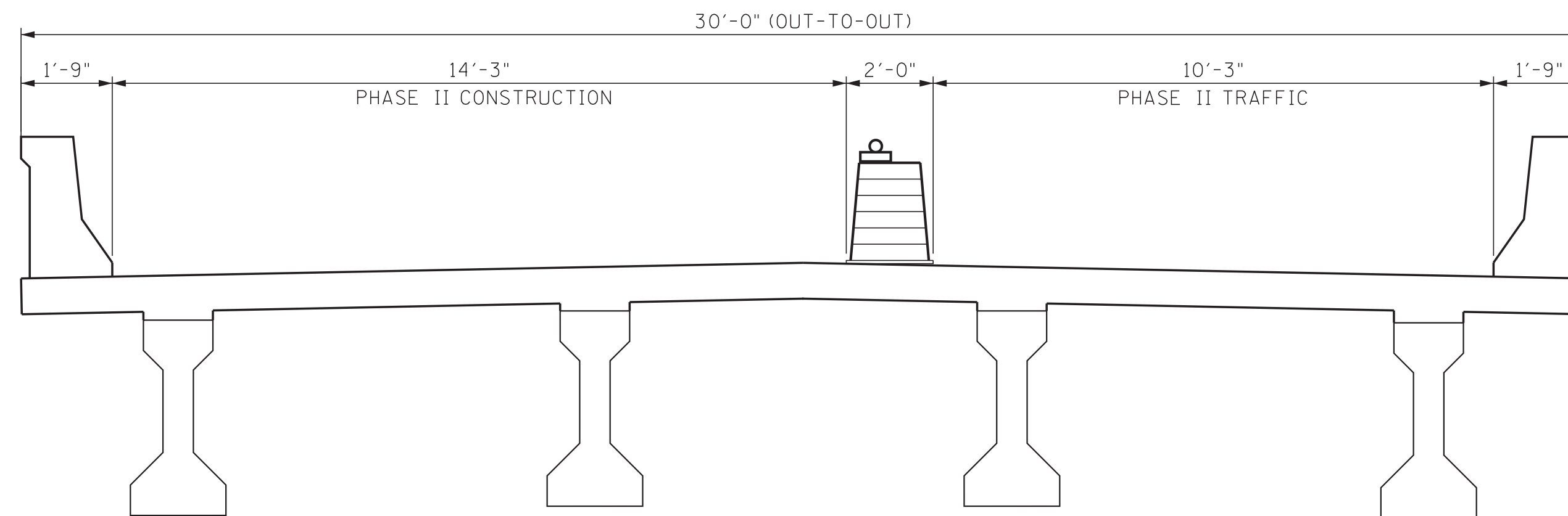


|               |      |           |
|---------------|------|-----------|
| PROJECT NO.   | YEAR | SHEET NO. |
| 39S201-M3-003 | 2025 | B6        |

[illegible]

(39-SR201-6.97)

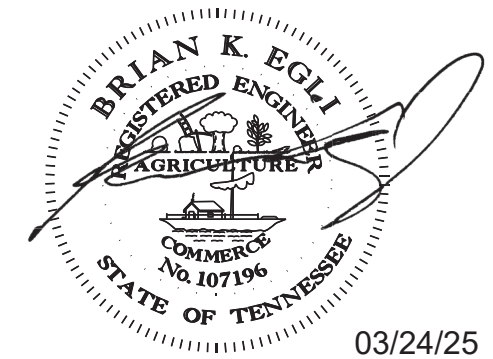
(LOOKING AHEAD ON SURVEY)



(39-SR201-6.97)

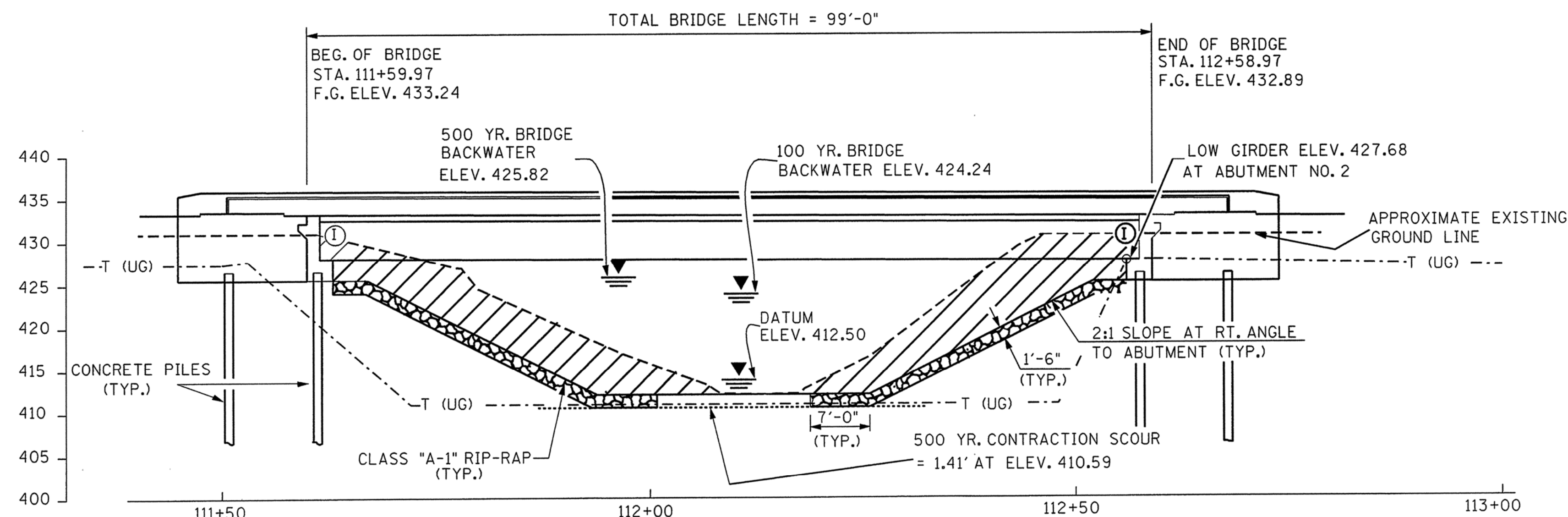
(LOOKING AHEAD ON SURVEY)

DESIGNED BY \_\_\_\_\_ DATE \_\_\_\_\_  
 DRAWN BY Z.HAYNES DATE 10/24  
 SUPERVISED BY K. MARTINKO DATE 10/24  
 CHECKED BY \_\_\_\_\_ DATE \_\_\_\_\_



B6





 DENOTES: EXISTING EXCAVATION AREA  
TO BE REMOVED AS A ROADWAY ITEM

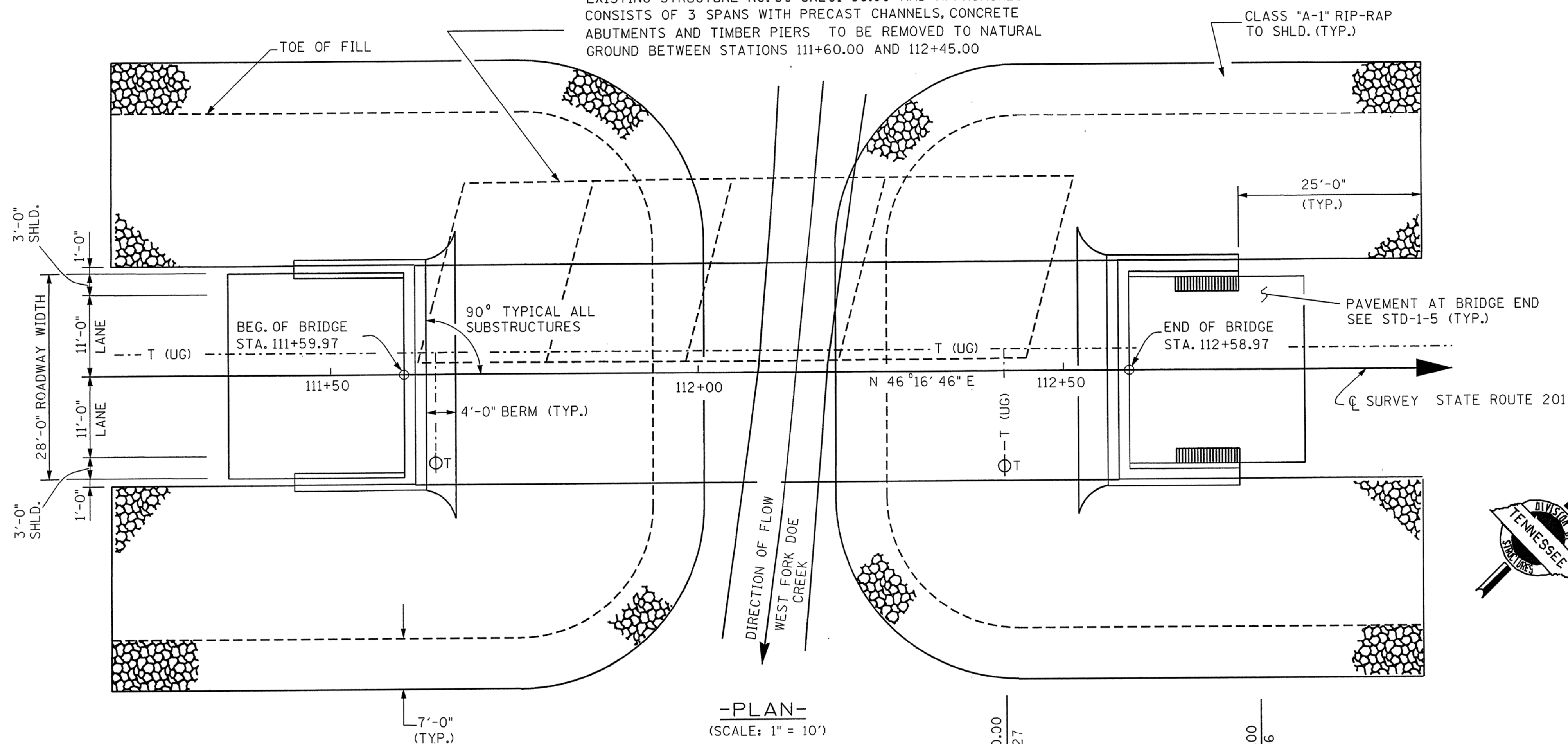
(I) DENOTES: INTEGRAL

-ELEVATION-  
(SCALE: 1" = 10')

NOTE: THE UNDERGROUND TELEPHONE LINE -T (UG)- IS APPROXIMATELY 3'  
LEFT OF THE PROPOSED CENTERLINE SURVEY

EXISTING STRUCTURE NO. 39-SR201-06.98 AND APPROACHES  
CONSISTS OF 3 SPANS WITH PRECAST CHANNELS, CONCRETE  
ABUTMENTS AND TIMBER PIERS TO BE REMOVED TO NATURAL  
GROUND BETWEEN STATIONS 111+60.00 AND 112+45.00

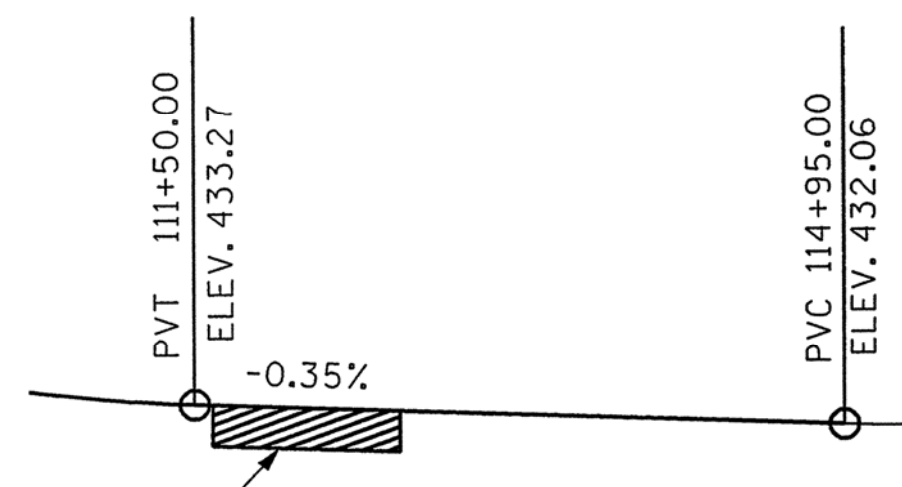
CLASS "A-1" RIP-RAP  
TO SHLD. (TYP.)



 DENOTES: END OF BRIDGE DRAIN (2' X 8'),  
SEE STD DWG NO. STD-1-6, 7, & 8

⊕ DENOTES: TELEPHONE JUNCTION BOX

-PLAN-  
(SCALE: 1" = 10')



NOTE: ANY EXCAVATION OF THE STREAM CHANNEL AREA (E.G., FOR PIER FOOTING OR RIP-RAP PLACEMENT) SHALL BE SEPARATED FROM FLOWING WATER, AND PERFORMED DURING LOW-FLOW CONDITIONS. THIS SHALL BE ACCOMPLISHED BY THE USE OF COFFERDAMS, FLUMES, LINED DIVERSION CHANNEL WITH SAND BAG BERM, OR DIVERSION PIPE WITH SAND BAG DAM AT PIPE INLET.

GRADE SKETCH  
(STATE ROUTE 201)

### HYDRAULIC DATA

DRAINAGE AREA = 2.97 mi<sup>2</sup>  
 DESIGN DISCHARGE 100 YR. = 1,988 cfs  
 WATER AREA PROVIDED BELOW EL. 424.24 = 469.01 ft<sup>2</sup>  
 100 YR. VELOCITY = 4.24 fps  
 100 YR. BRIDGE BACKWATER = 0.00 ft @ EL. 424.24  
 ROADWAY OVERTOPPING EL. = 432.00

[illegible]

## LIST OF DRAWINGS

| TITLE                                | DWG. NO.  | LAST REV. DATE |
|--------------------------------------|-----------|----------------|
| LAYOUT OF BRIDGE                     | M-442-96  | 3-6-03         |
| GENERAL NOTES & ESTIMATED QUANTITIES | M-442-97  | <b>3-6-03</b>  |
| FOUNDATION DATA                      | M-442-98  |                |
| SUPERSTRUCTURE                       | M-442-99  |                |
| SUPERSTRUCTURE DETAILS               | M-442-100 |                |
| SUPERSTRUCTURE DETAILS               | M-442-101 | 3-6-03         |
| PRESTRESSED I-BEAM DETAILS           | M-442-102 |                |
| ABUTMENT NO. 1                       | M-442-103 | 3-6-03         |
| ABUTMENT NO. 2                       | M-442-104 | 3-6-03         |
| ABUTMENT NO. 1 & 2 DETAILS           | M-442-105 | 3-6-03         |
| FINAL FOUNDATION DATA                | M-442-106 |                |
| BILL OF STEEL                        | M-442-107 |                |

## LIST OF STANDARD DRAWINGS

| LIST OF STANDARD DRAWINGS   |          | LAST<br>REV. DATE |
|---|----------|-------------------|
| TITLE   | DWG. NO. |                   |
| BRIDGE RAILING CONCRETE PARAPET                                     | STD-1-1  | 7-31-00           |
| PAVEMENT AT BRIDGE ENDS   | STD-1-5  | 7-31-00           |
| BRIDGE END DRAIN W/PABE   | STD-1-6  | 4-28-97           |
| BRIDGE END DRAIN W/PABE   | STD-1-7  | 7-31-00           |
| BRIDGE END DRAIN 2' X 8"-7" W/PABE                                  | STD-1-8  | 5-01-95           |
| STD. PRECAST PRESTRESSED BRIDGE<br>DECK PANELS GENERAL DETAILS      | STD-4-1  | 5-21-99           |
| STD. PRECAST PRESTRESSED BRIDGE<br>DECK PANELS DESIGN CRITERIA      | STD-4-2  | 6-10-96           |
| STD. PRECAST PRESTRESSED BRIDGE<br>DECK PANELS GENERAL DETAILS      | STD-4-3  | 6-10-96           |
| STD. PRECAST PRESTRESSED BRIDGE<br>DECK PANELS CONSTRUCTION DETAILS | STD-4-4  | 6-10-96           |
| STD. PILE DETAILS   | STD-5-1  | 10-25-93          |
| STD. PILE DETAILS   | STD-5-2  | 5-21-99           |
| REINFORCING BAR SUPPORT DETAILS<br>FOR CONCRETE SLABS               | STD-9-1  | 12-19-94          |
| MISCELLANEOUS ABUTMENT AND<br>DRAINAGE DETAILS                      | STD-10-1 | 5-11-92           |
| STD. DETAILS AND INT. DIAPH. DETAILS<br>FOR I-BEAMS                 | STD-14-2 | 7-31-00           |

## LIST OF SPECIAL PROVISIONS

| REGARDING                      | DWG. NO.  | LAST REV. DATE |
|--------------------------------|-----------|----------------|
| APPROVAL OF SHOP DRAWINGS----- | 105A----- | 12-15-97       |

2023 ADT= 1,111  
28'-0" ROADWAY WITH STD-1-1 PARAPET  
DESIGN SPEED = 50 MPH

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION

LAYOUT  
STATE ROUTE 201  
OVER

WEST FORK DOE CREEK  
BRIDGE I.D. NO. 39S81350007  
STATION 112+09.47 L.M. 6.98  
HENDERSON COUNTY  
2003

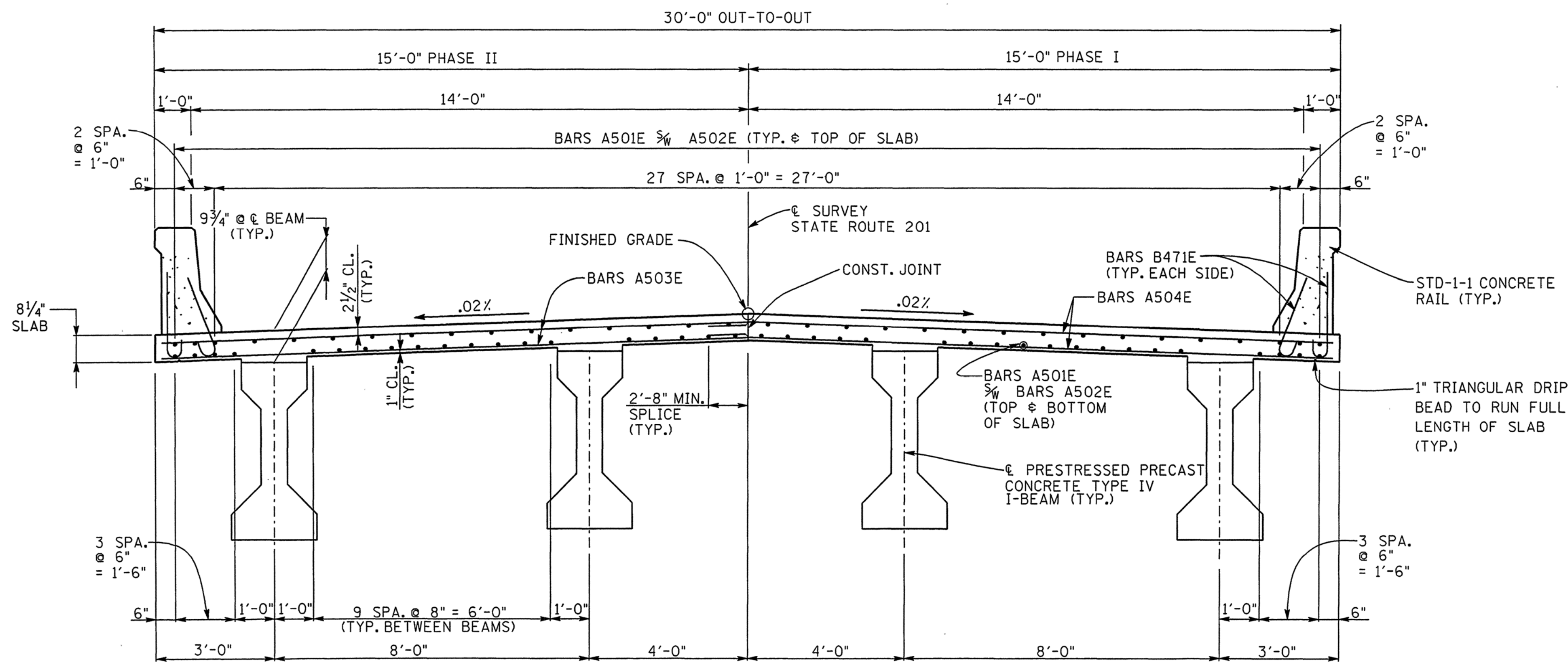
CORRECT Edward P. Wasserman  
ENGINEER OF STRUCTURES

M-442-96

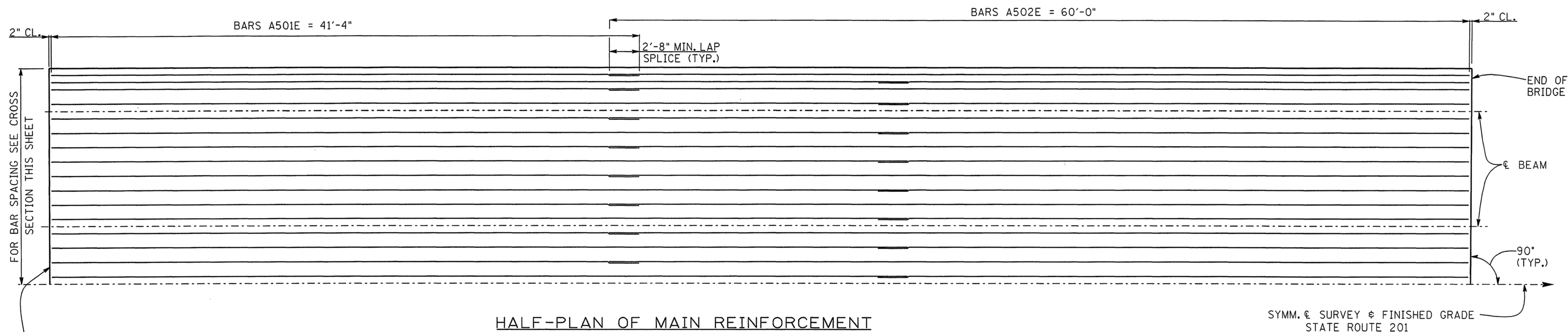
DESIGNED BY JEFF COYNE / AE DATE 05/02  
DRAWN BY JEFF COYNE(RKD) DATE 11/02  
SUPERVISED BY RLH/TET DATE 11/02  
CHECKED BY \_\_\_\_\_ DATE \_\_\_\_\_

TEMPORARY MEDIAN BARRIER RAIL = 147'-0" CLASS "A-1" RIP-RAP = 900 TONS



[illegible]

TYPICAL CROSS SECTION



### HALF-PLAN OF MAIN REINFORCEMENT

### ESTIMATED QUANTITIES

|   |   |
|---|---|
| CLASS 'D'<br>CONCRETE<br>(BRIDGE<br>DECK)<br>C.Y. | EPOXY COATED<br>REINFORCING<br>STEEL<br>LB. |
| 76  | 17,816                                      |

SYMM. & SURVEY & FINISHED GRADE  
STATE ROUTE 201

STATE OF TENNESSEE  
DEPARTMENT OF TRANSPORTATION  
SUPERSTRUCTURE  
STATE ROUTE 201  
OVER  
WEST FORK DOE CREEK  
STATION 112+09.47  
LOG MILE 6.98  
HENDERSON COUNTY  
2003



CORRECT Edward P. Wasserman  
ENGINEER OF STRUCTURES

DESIGNED BY J. COYNE DATE 05-02  
 DRAWN BY R. DALE DATE 10-02  
 SUPERVISED BY RLH/ TET DATE 10-02  
 CHECKED BY J. COYNE DATE 12-02